

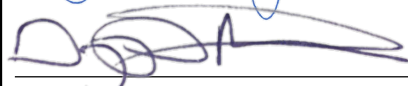
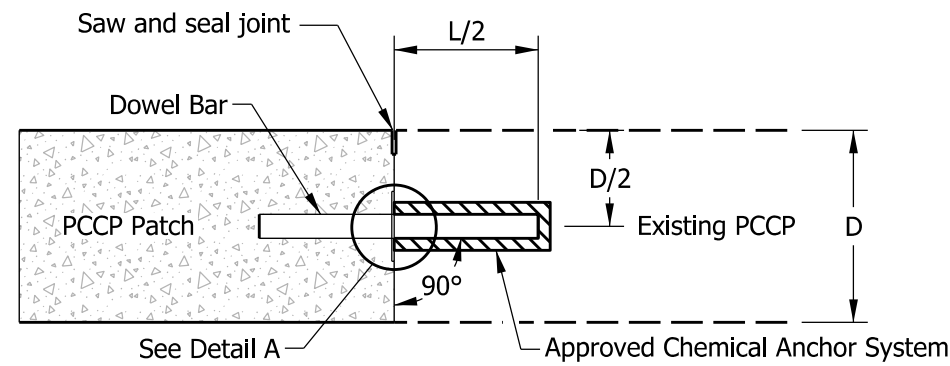
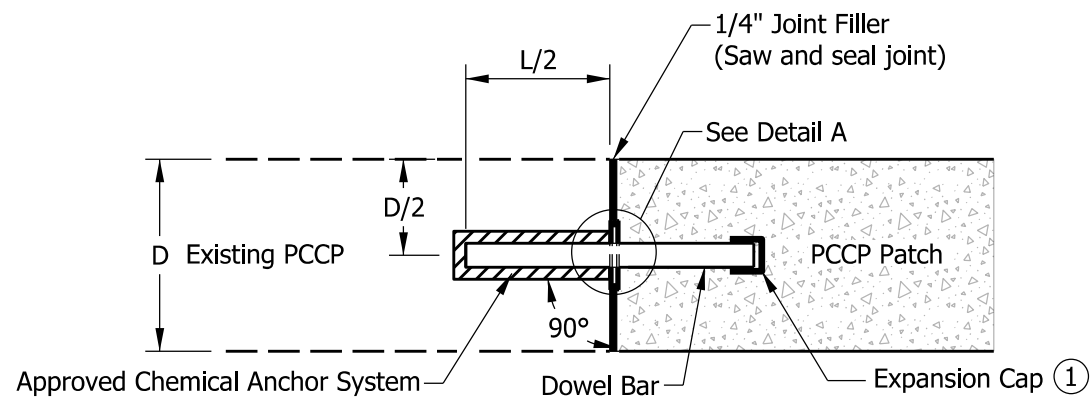


INDEX	
SHEET NO.	SUBJECT
1	Concrete Pavement Patching Index and General Notes
2	Joint Details
3	Joint Placement
4	Patch Length \geq 6' and \leq 15'
5	Patch Length $>$ 15' and \leq 60'
6	Patch Length $>$ 60'
7	Dowel Alignment and Saw Cut Tolerances

GENERAL NOTES:

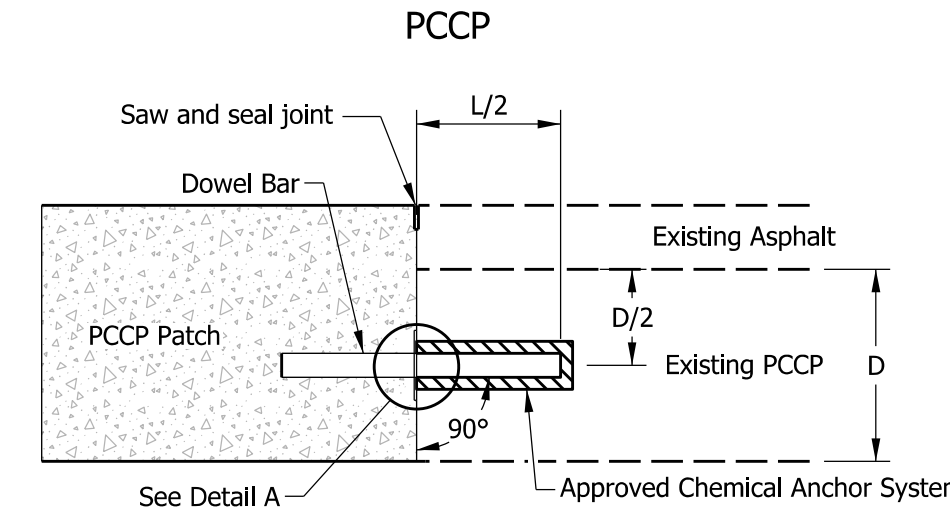
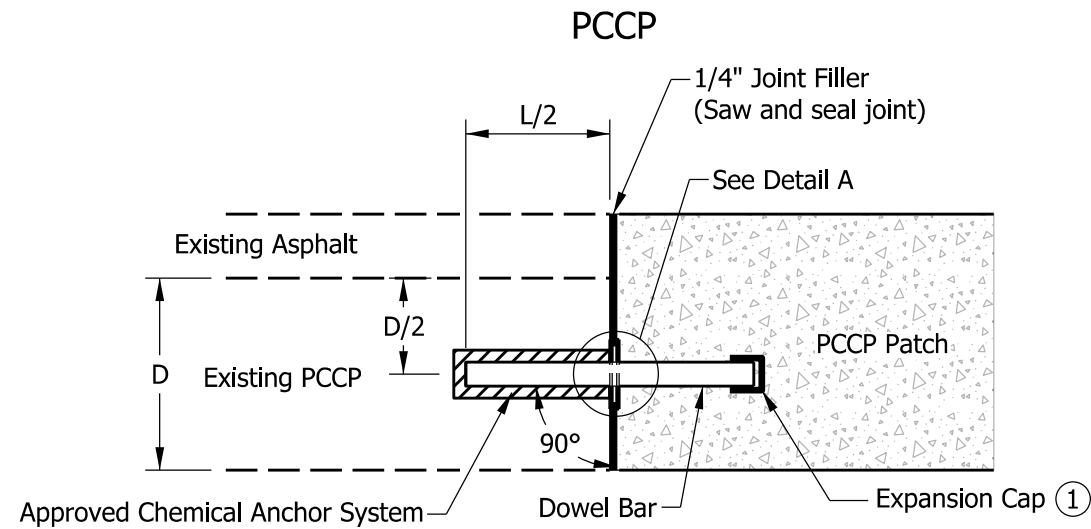
1. Dowel bars shall be epoxy coated.
2. Tie-bars shall be epoxy coated.
3. Additional preparation of existing subgrade will be determined by the Engineer.
4. See Standard Drawing E 503-CCPJ-02 for sawed joint and joint sealant details.
5. See Standard Drawing E 503-CCPJ-03 for D-1 contraction joint details.
6. See Standard Drawing E 503-CCPJ-05 for retrofitted tie-bar details.
7. The minimum patch length shall be 6 ft.

INDIANA DEPARTMENT OF TRANSPORTATION	
CONCRETE PAVEMENT PATCHING INDEX AND GENERAL NOTES	
SEPTEMBER 2020	
STANDARD DRAWING NO. E 506-CCPP-01	
	
	DESIGN STANDARDS ENGINEER 03/10/20 <small>DATE</small>
	
CHIEF ENGINEER	05/01/20 <small>DATE</small>



NOTE:

- ① Expansion cap shall be placed with a gap of 1/4 in. minimum between end of dowel bar and cap.
2. Dowel bar diameter shall be as follows:
 - 1 in. for existing PCCP thickness 10 in. or less
 - 1.5 in. for existing PCCP thickness greater than 10 in.
3. Dowel bar length shall be 1 ft 2 in. minimum and 1 ft 6 in. maximum, regardless of dowel diameter.
4. Sawing and sealing joints shall be omitted where the concrete patch is to be overlaid with asphalt or concrete.

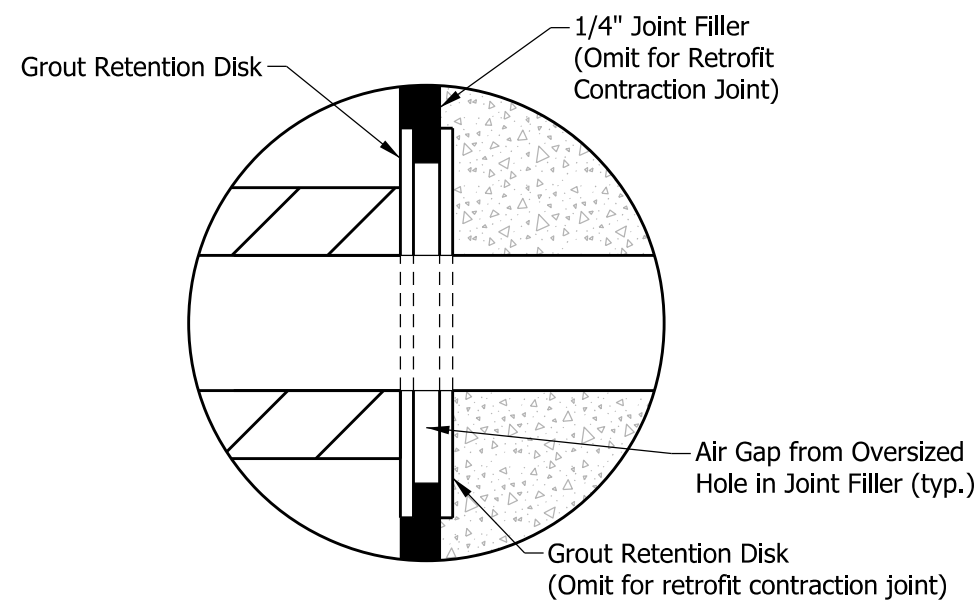


**COMPOSITE PAVEMENT
RETROFIT PRESSURE RELIEF JOINT**

**COMPOSITE PAVEMENT
RETROFIT CONTRACTION JOINT**

LEGEND

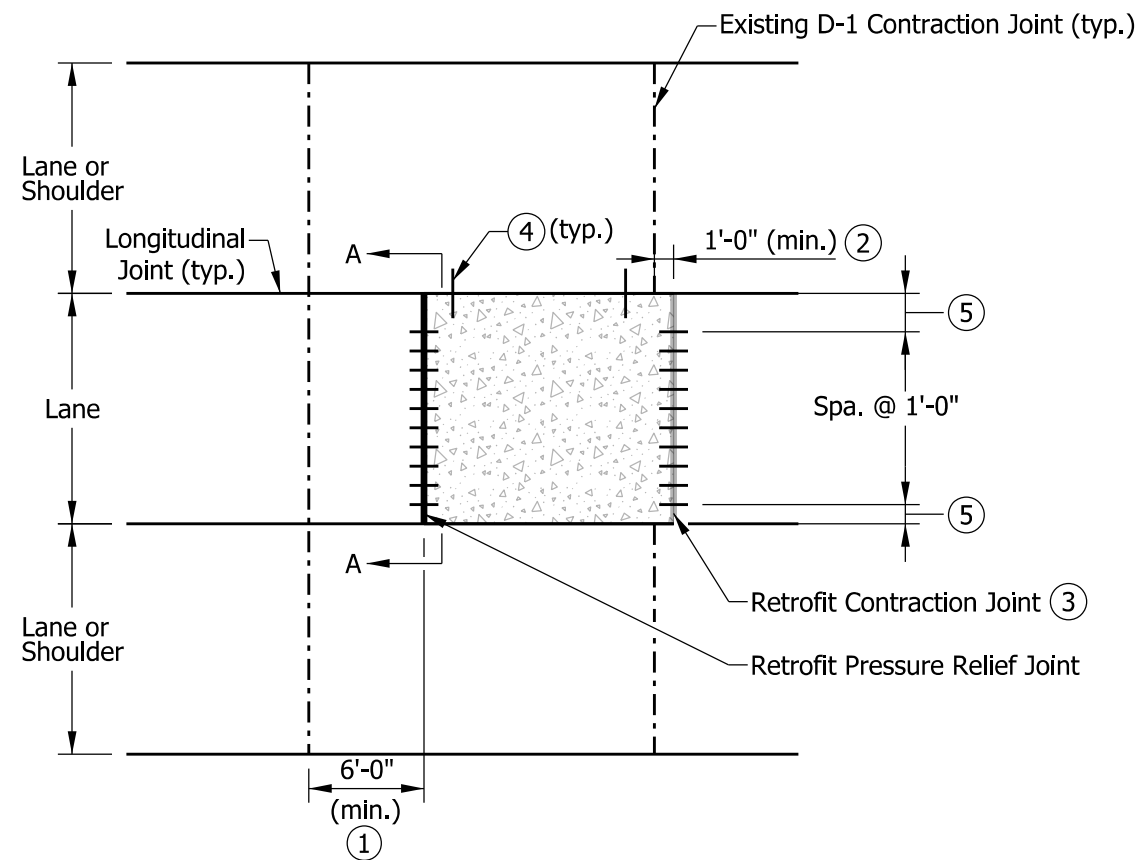
D = Existing PCCP Thickness
L = Dowel Bar Length



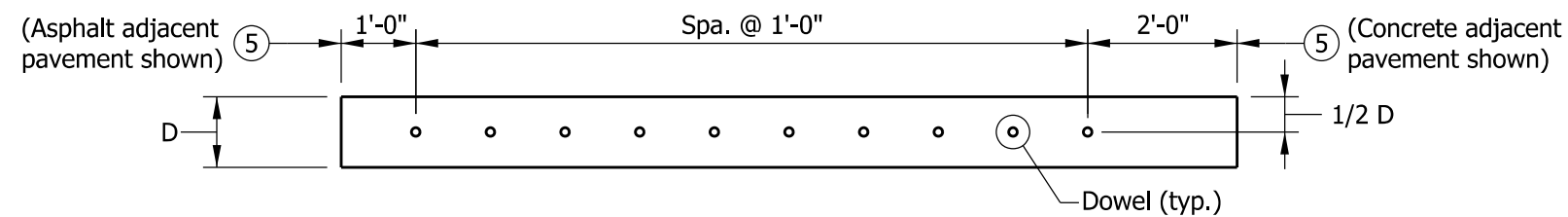
DETAIL A

(Retrofit Pressure Relief Joint shown
Retrofit Contraction Joint same by opposite hand)

INDIANA DEPARTMENT OF TRANSPORTATION	
JOINT DETAILS	
SEPTEMBER 2020	
STANDARD DRAWING NO.	E 506-CCPP-02
	 DESIGN STANDARDS ENGINEER 03/10/20 DATE
	 CHIEF ENGINEER 05/01/20 DATE



PLAN



SECTION A-A

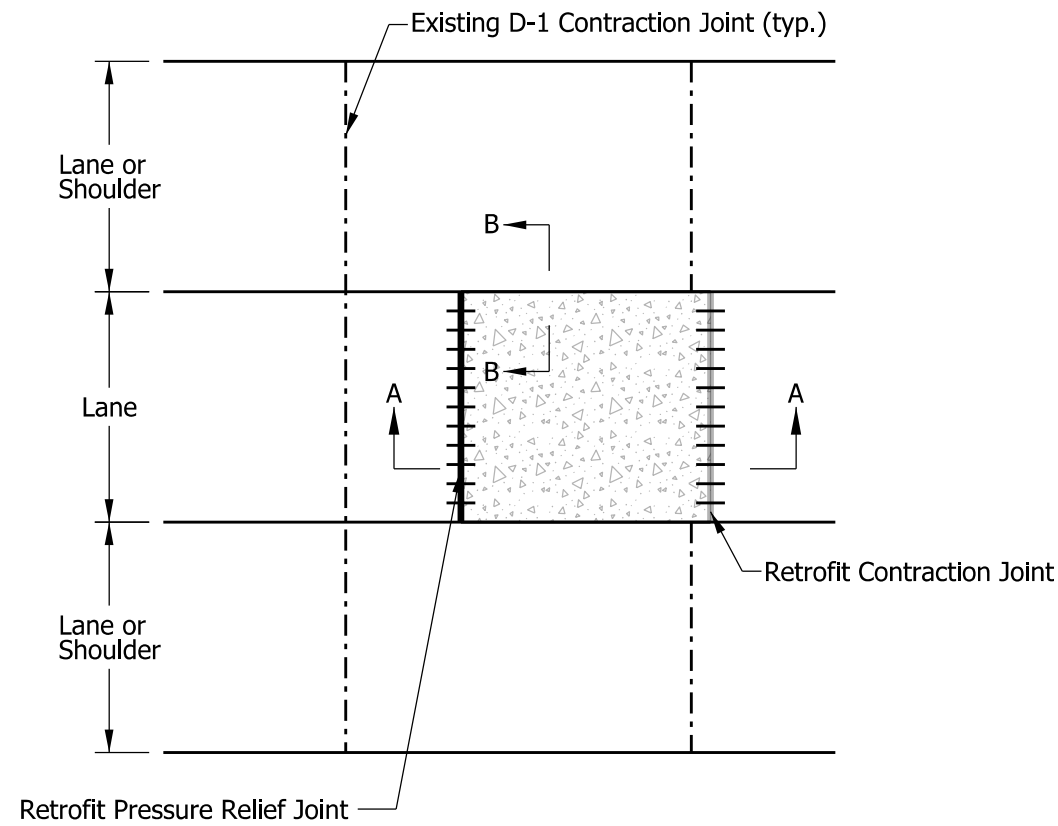
NOTES:

- ① Retrofit pressure relief joints and retrofit contraction joints shall be placed a minimum of 6 ft from an existing D-1 contraction joint located in the same lane as the patch.
- ② Retrofit pressure relief joints and retrofit contraction joints shall be placed a minimum of 1 ft from an existing D-1 contraction joint located in a lane adjacent to the patch.
- ③ Where the total patch length exceeds 60 ft, a retrofit pressure relief joint shall be used in lieu of the retrofit contraction joint.
- ④ Retrofitted tie-bars as required for patches greater than 15 ft.
- ⑤ Distance to first dowel shall be as follows:
 - 1 ft 0 in. where adjacent pavement is asphalt
 - 2 ft 0 in. where adjacent pavement is concrete

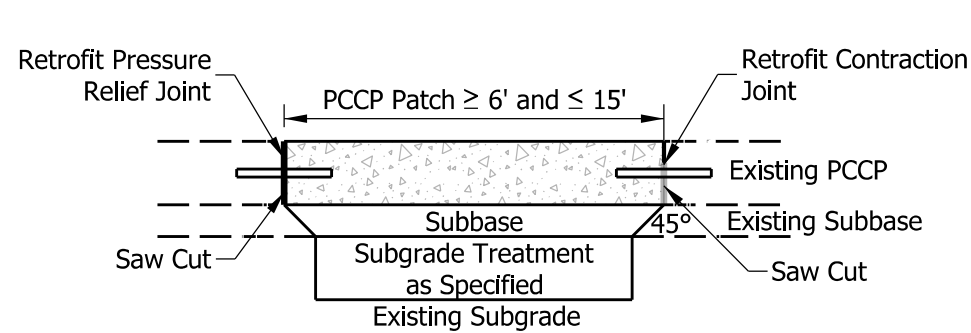
LEGEND

D = Existing PCCP Thickness

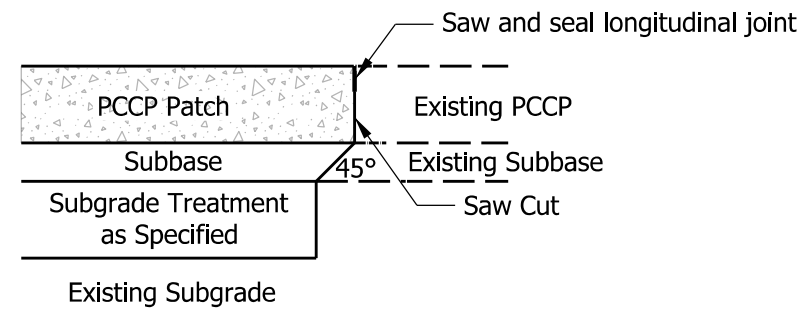
INDIANA DEPARTMENT OF TRANSPORTATION	
JOINT PLACEMENT	
SEPTEMBER 2020	
STANDARD DRAWING NO.	E 506-CCPP-03
	 DESIGN STANDARDS ENGINEER 03/10/20 DATE
	 CHIEF ENGINEER 05/01/20 DATE



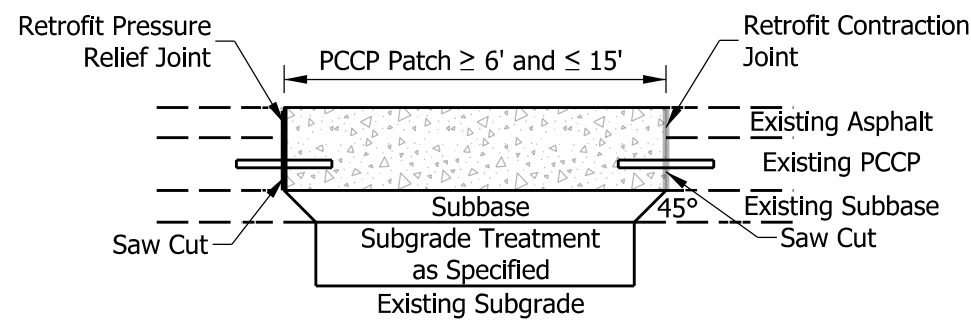
PLAN



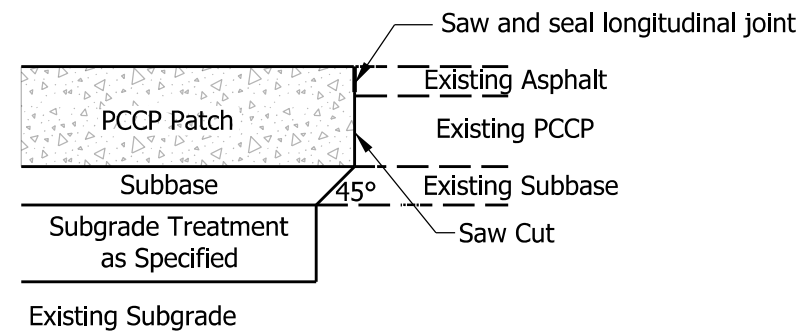
PCCP SECTION A-A



PCCP SECTION B-B

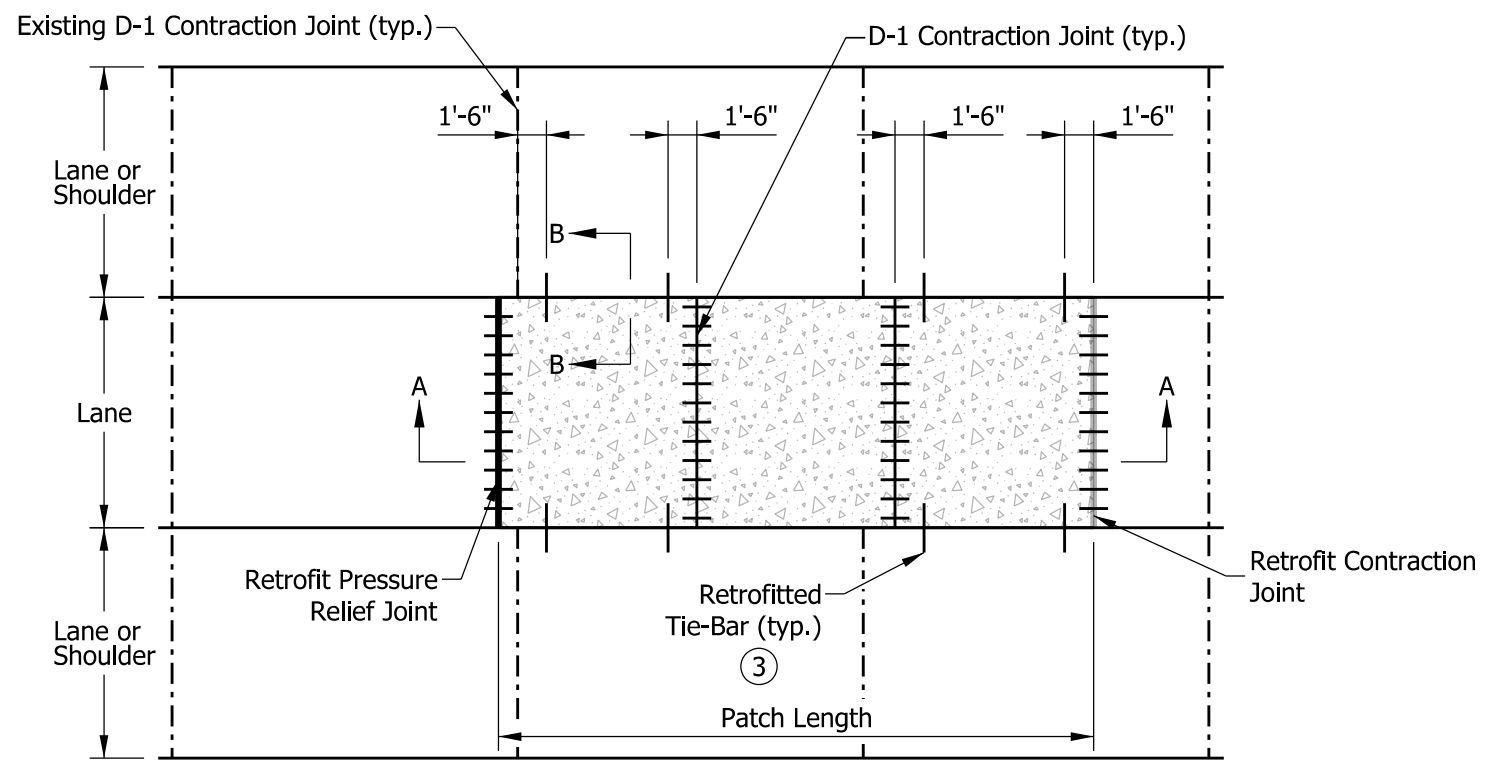


COMPOSITE PAVEMENT SECTION A-A



COMPOSITE PAVEMENT SECTION B-B

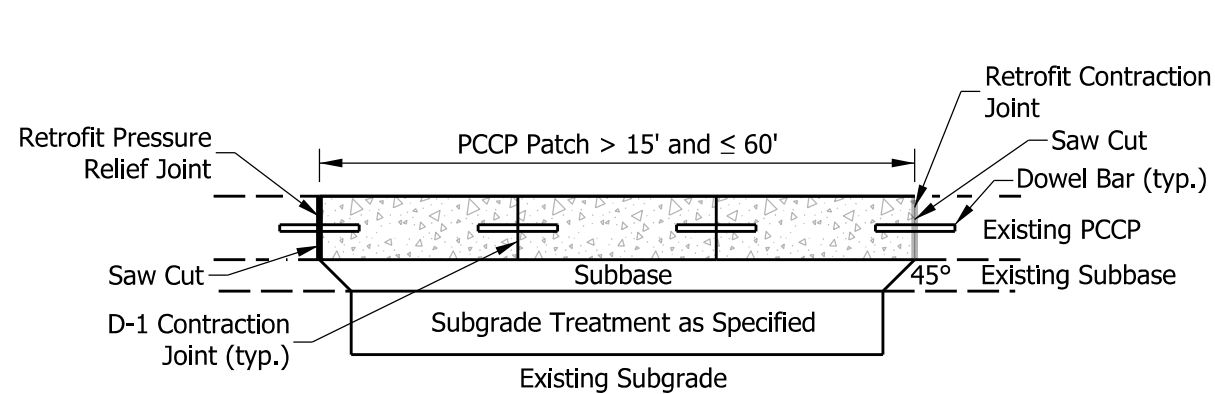
INDIANA DEPARTMENT OF TRANSPORTATION	
PATCH LENGTH $\geq 6'$ AND $\leq 15'$	
SEPTEMBER 2020	
STANDARD DRAWING NO.	E 506-CCPP-04
	 DESIGN STANDARDS ENGINEER 03/10/20 DATE
	 CHIEF ENGINEER 05/01/20 DATE



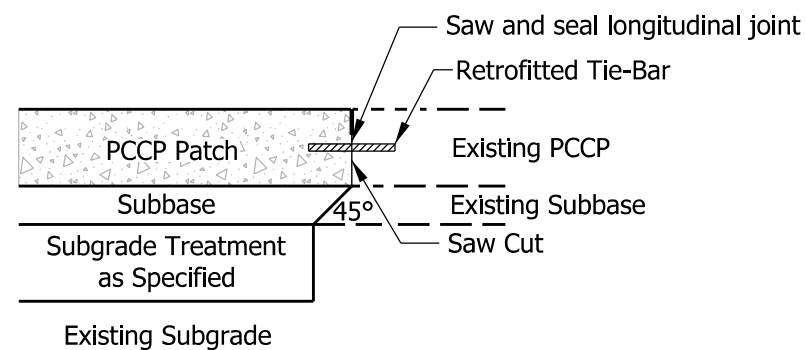
PLAN

NOTES:

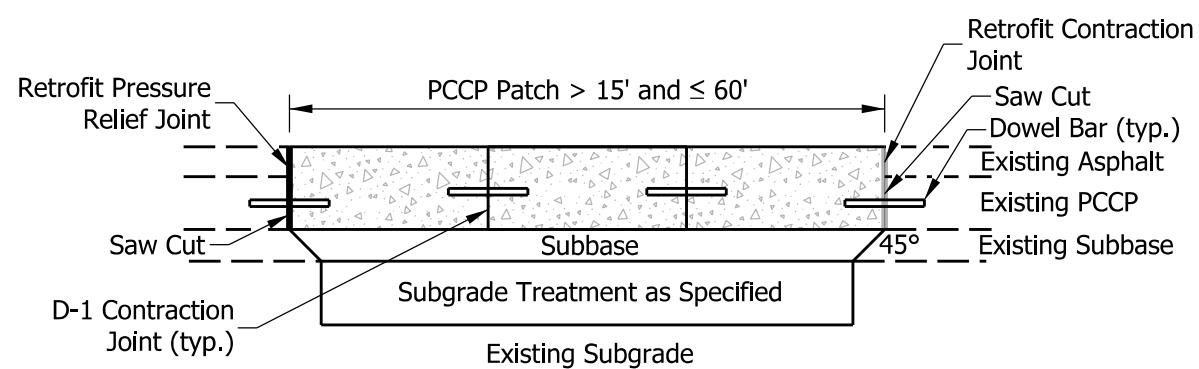
1. D-1 contraction joints shall be spaced at 15 ft. Where 15 ft spacing results in the last panel being less than 6 ft in length, the last D-1 spacing shall be adjusted to create two equal panel lengths greater than 6 ft.
2. Retrofitted tie-bars shall be placed in every other panel as shown.
- ③ Retrofitted tie-bars shall be used where adjacent lane or shoulder is PCCP or composite pavement.



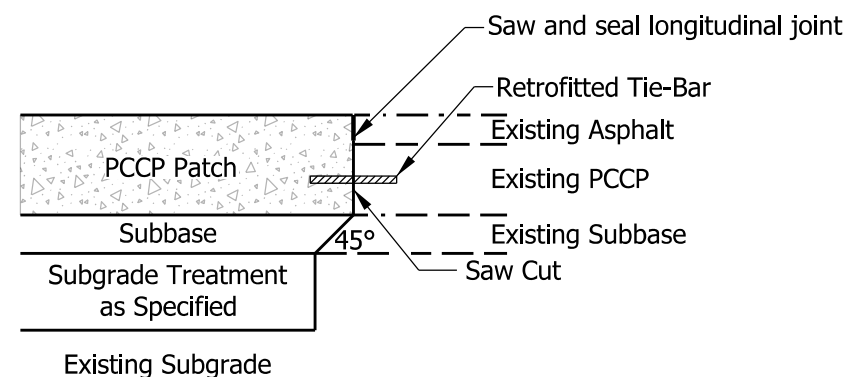
PCCP SECTION A-A



PCCP SECTION B-B

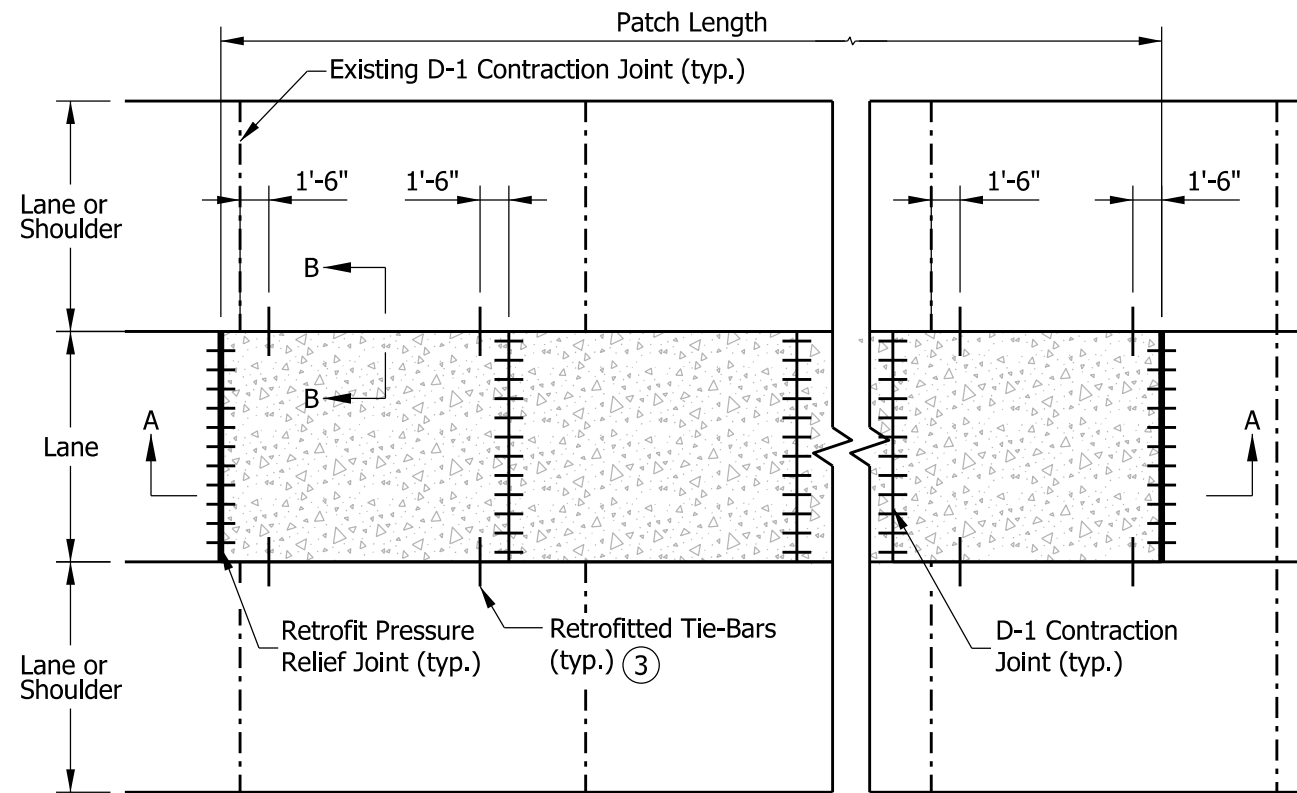


COMPOSITE PAVEMENT SECTION A-A



COMPOSITE PAVEMENT SECTION B-B

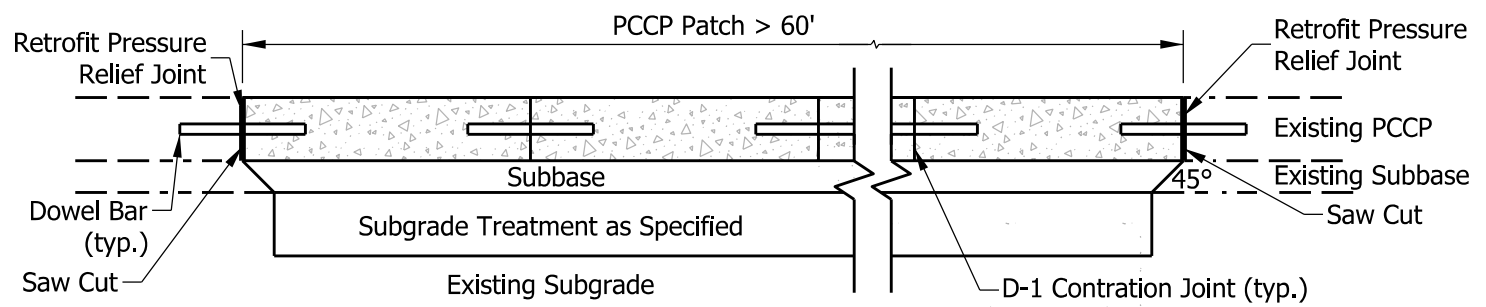
INDIANA DEPARTMENT OF TRANSPORTATION	
PATCH LENGTH > 15' AND ≤ 60'	
SEPTEMBER 2020	
STANDARD DRAWING NO. E 506-CCPP-05	
	 DESIGN STANDARDS ENGINEER 03/10/20 DATE
	 CHIEF ENGINEER 05/01/20 DATE



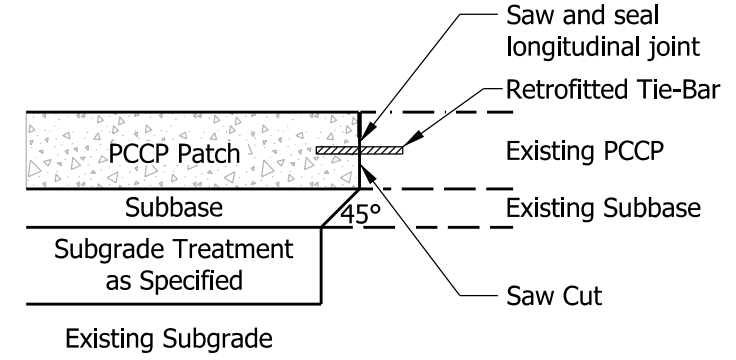
PLAN

NOTES:

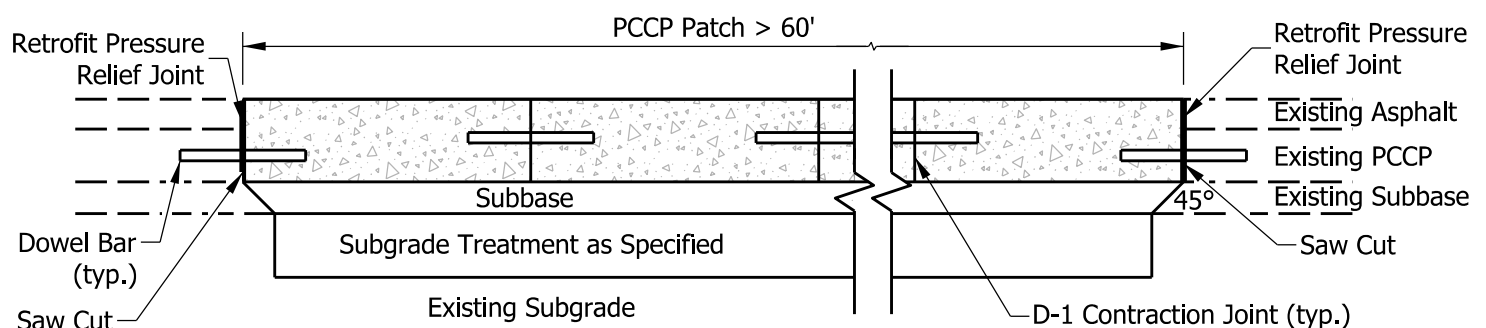
1. D-1 contraction joints shall be spaced at 15 ft. Where 15 ft spacing results in the last panel being less than 6 ft in length, the last D-1 spacing shall be adjusted to create two equal panel lengths greater than 6 ft.
2. Retrofitted tie-bars shall be placed in every other panel as shown.
- ③ Retrofitted tie-bars shall be used where adjacent lane or shoulder is PCCP or composite pavement.



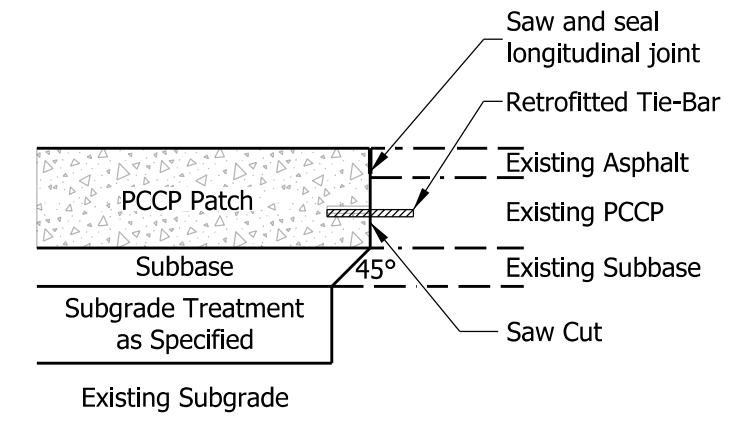
PCCP SECTION A-A



PCCP SECTION B-B

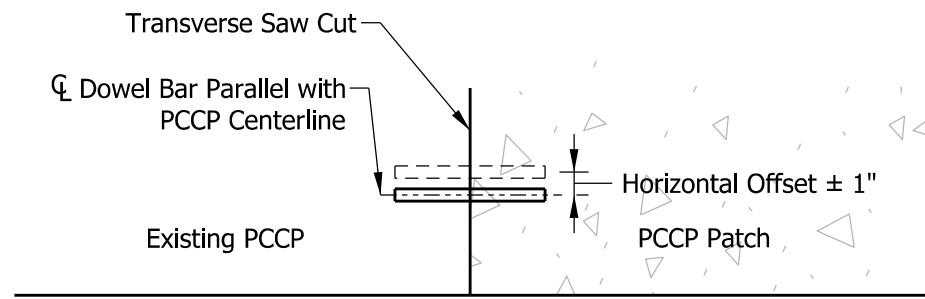


COMPOSITE PAVEMENT SECTION A-A

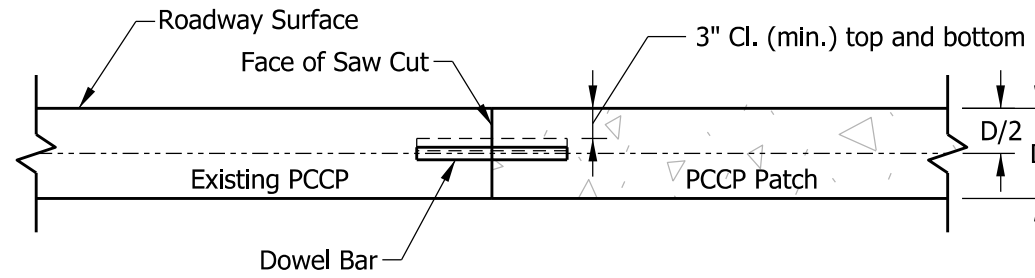


COMPOSITE PAVEMENT SECTION B-B

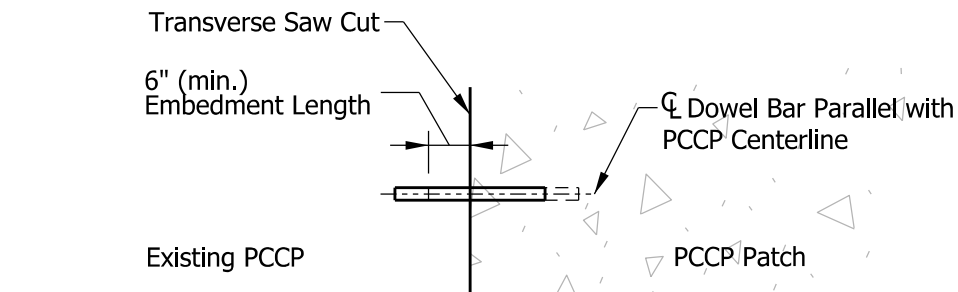
INDIANA DEPARTMENT OF TRANSPORTATION	
PATCH LENGTH > 60'	
SEPTEMBER 2020	
STANDARD DRAWING NO. E 506-CCPP-06	
	 DESIGN STANDARDS ENGINEER 03/10/20 DATE
	 CHIEF ENGINEER 05/01/20 DATE



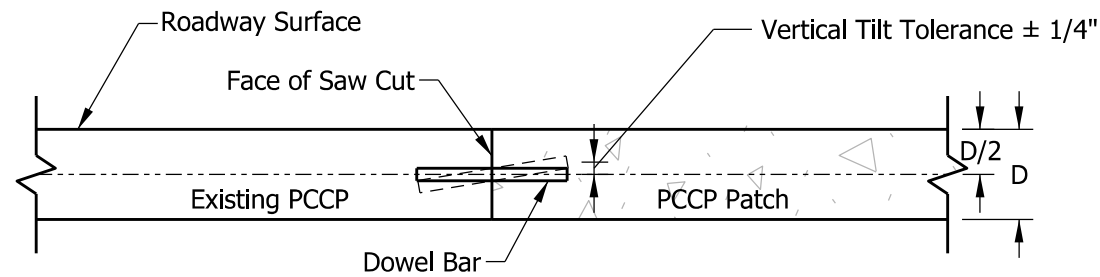
PLAN HORIZONTAL TRANSLATION



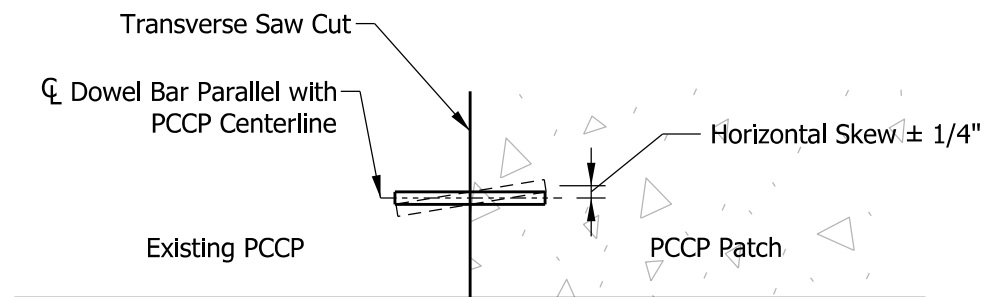
ELEVATION VERTICAL TRANSLATION



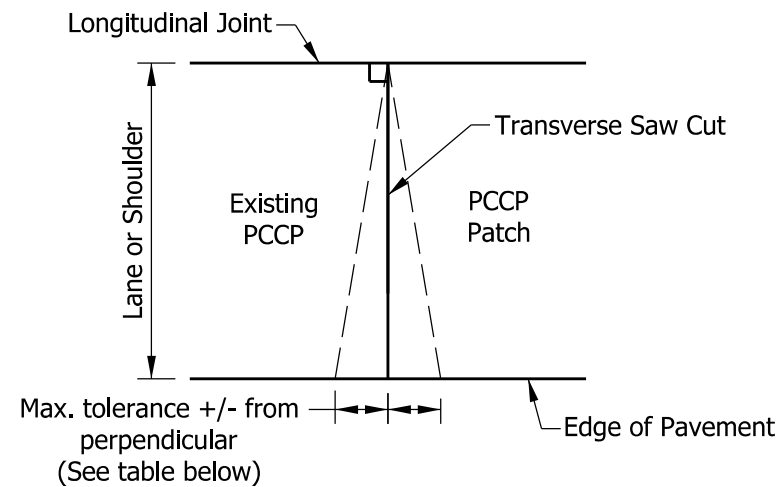
PLAN LONGTITUDINAL TRANSLATION



ELEVATION VERTICAL TILT



PLAN HORIZONTAL SKEW



WIDTH OF LANE OR SHOULDER	MAX. TOLERANCE
10'	1 5/8"
12'	2"
14'	2 5/16"

PLAN SAW CUT

LEGEND

- D = Existing PCCP Thickness
- Mis-Aligned Dowel Bar
- Properly Aligned Dowel Bar


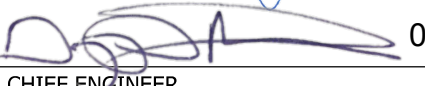
INDIANA DEPARTMENT OF TRANSPORTATION

DOWEL ALIGNMENT AND SAWCUT TOLERANCES

SEPTEMBER 2020

STANDARD DRAWING NO. E 506-CCPP-07



 03/10/20
 DESIGN STANDARDS ENGINEER DATE
 05/01/20
 CHIEF ENGINEER DATE